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HDR ENGINEERING, INC.
120 BRENTWOOD COMMONS WAY
SUITE 525
BRENTWOOD, TN 37027
ANTHONY L. WASHINGTON, III, P.E. NO. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B
GENERAL NOTES	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES	2E, 2E1
TABULATED QUATITIES	2F
UTILITY NOTES AND UTILITY OWNERS	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	4

YEAR	PROJECT NO.	SHEET NO.
2025	HSIP-22(102)	ROADWAY-SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

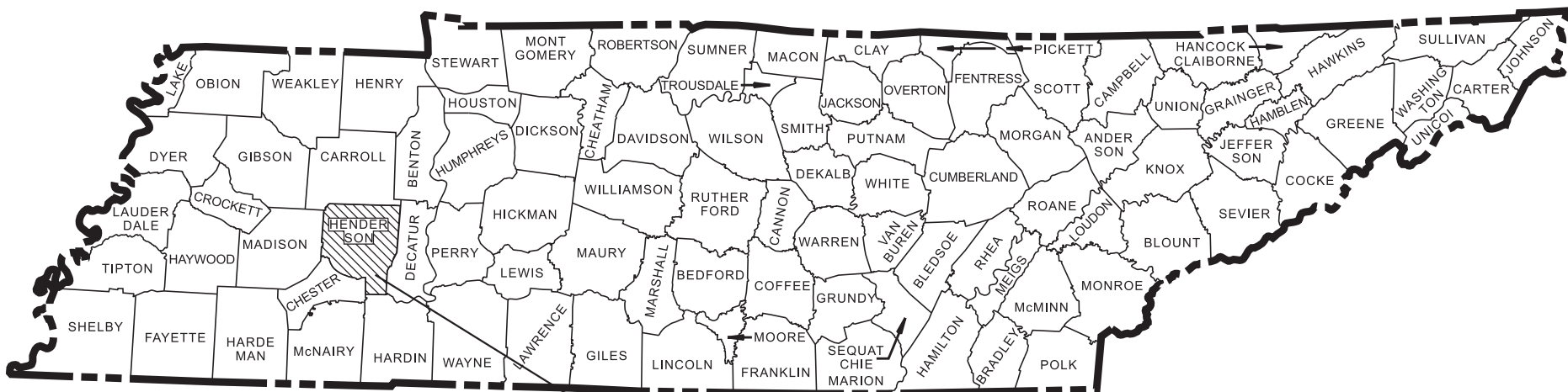
HENDERSON COUNTY

S.R. 22
FROM: L.M. 0.00 (CHESTER COUNTY LINE)
TO: L.M. 8.98 (NEAR TAYLOR BAILEY ROAD)

RESURFACE & SAFETY
MILL & 411D @ 159 LB/SY, PAVEMENT MARKINGS, AND GUARDRAIL
STATE HIGHWAY NO. 22 F.A.H.S. NO.

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	STP/HSIP-22(102)	
STATE PROJ. NO.	39S022-F8-004	
STATE PROJ. NO.	39S022-F3-004	
STATE PROJ. NO.	39S022-M3-004	



PROJECT LOCATION
BRIDGE ID. # 39SR0220001 39SR0220003 39SR0220005

NO EXCLUSIONS

39S022-F8-004
39S022-F3-004
END PROJECT NO. STP/HSIP-22(102) RESURFACE & SAFETY
L.M. 8.98 (NEAR TAYLOR BAILEY ROAD)

BRIDGE DECK REPAIR PROJECT NO. 39S022-M3-004
SR-22 @ L.M. 6.07

39S022-F8-004
39S022-F3-004
BEGIN PROJECT NO. STP/HSIP-22(102) RESURFACE & SAFETY
L.M. 0.00 (CHESTER COUNTY LINE)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

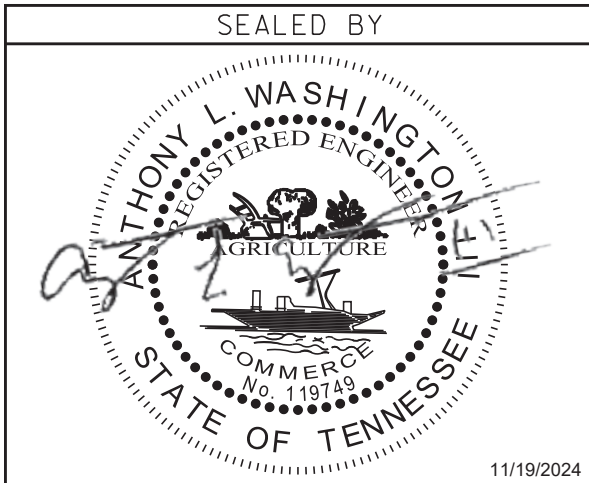
TDOT PROJECT MANAGER: LYNN EVANS, P.E., REG. 4
DESIGNED BY : HDR ENGINEERING, INC.
DESIGNER : ANTHONY L. WASHINGTON, III, P.E. CHECKED BY DAVID HORNE, P.E.
P.E. NO. 98043-4283-04
PIN NO. 135283.00



ROADWAY LENGTH 8.98 MILES
TOTAL LANE MILES RESURFACED 17.93 MILES

TRAFFIC COUNTER & WEATHER STATIONS	
STATION LOCATION	LOG MILE
TC STATION 62	1.219
TC STATION 70	8.655

TRAFFIC DATA	
ADT (2025)	8,344
POSTED SPEED LIMITS	
L.M. 0.00 TO L.M. 8.98	55 MPH



APPROVED: WILL REID, CHIEF ENGINEER
DATE:
APPROVED: HOWARD H. ELEY, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: DIVISION ADMINISTRATOR DATE

ROADWAY INDEX

STANDARD ROADWAY DRAWINGS

SHEET NAME	SHEET NO.	DWG.	REV.	DESCRIPTION
SIGNATURE SHEET.....	ROADWAY-SIGN1			
TITLE SHEET.....	1			
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS.....	1A	RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
PROJECT COMMITMENTS.....	1B	RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
ESTIMATED ROADWAY QUANTITIES.....	2	RD-L-1	02-20-20	STANDARD LEGEND
TYPICAL SECTIONS AND PAVEMENT SCHEDULE.....	2B	RD-L-1A		STANDARD LEGEND
GENERAL NOTES.....	2C			
SPECIAL NOTES	2D	S-GRS-4	05-04-22	SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL
ENVIRONMENTAL NOTES.....	2E, 2E1	S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
TABULATED QUANTITIES.....	2F	S-GRT-2R	06-28-19	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL (RETROFIT)
UTILITY NOTES AND UTILITY OWNERS.....	3			
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL....	4			
BRIDGE PLANS.....	B-1			
NO UTILITY SHEETS INCLUDED IN THE SET OF PLANS				

STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS

RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND

SAFETY DESIGN AND GUARDRAILS


S-GRS-4	05-04-22	SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
S-GRT-2R	06-28-19	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL (RETROFIT)

DESIGN - TRAFFIC CONTROL

T-M-1	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-3	07-07-23	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERSAND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	07-17-20	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-16	07-30-24	RUMBLE STRIPE INSTALLATION LAYOUT
T-M-16A	02-03-20	RUMBLE STRIPEDETAILSFOR EDGE OF PAVEMENT AND CENTERLINE
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	HSIP-22(102)	1A

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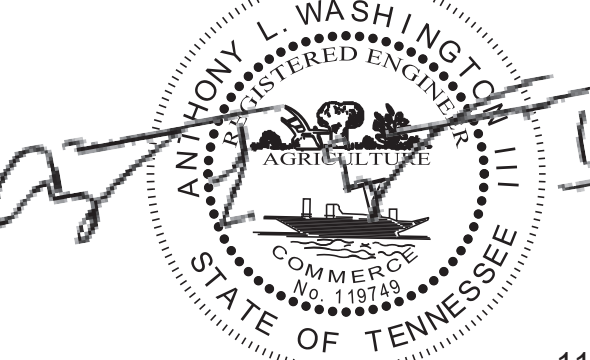
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX
AND
STANDARD
ROADWAY
DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	HSIP-22(102)	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION HAZARDOUS MATERAILS	ASBESTOS CONTAINING MATERIAL(ACM) SURVEY WAS COMPLETED ON BRIDGE NO. SR-22 OVER PINEY CREEK L.M. 6.07 (39-SR022-06.07). NO ASBESTOS WAS DETECTED. PLEASE SEE REPORTS FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03.	BRIDGE NO. SR-22 OVER PINEY CREEK L.M. 6.07 (39-SR022-06.07)

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11/19/2024

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS

ESTIMATED ROADWAY QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 39S022-M8-004	QUANTITY 39S022-F3-004	TOTAL QUANTITY
	203-06 WATER	MG	26		26
(1)	208-01.05 BROOMING & DEGRASSING SHOULDERS	L.M.	17.9		17.9
(2)	303-02 MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)	TON	3471		3471
(3)	307-02.01 ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING A	TON	1073		1073
(4)	403-02.01 TRACKLESS TACK COAT	TON	61		61
(5)(6)	411-01.21 LONGITUDINAL JOINT SEALANT	L.M.	9.2		9.2
(7)	411-02.10 ACS MIX (PG70-22) GRADING D	TON	11141		11141
	411-12.04 SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (4IN WIDTH)	L.M.	12.4		12.4
(8)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	10938		10938
(9)	705-02.10 GUARDRAIL TRANSITION 27IN TO 31IN	EACH		17	17
(9)	705-04.10 EARTH PAD FOR GUARD RAIL END TREATMENT	EACH		17	17
(9)	705-06.20 TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH		17	17
(9)	706-01 GUARDRAIL REMOVED	L.F.		850	850
(10)	712-01 TRAFFIC CONTROL	LS	1		1
(11)	712-06 SIGNS (CONSTRUCTION)	S.F.	2579		2579
	716-01.21 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH	684		684
	716-01.22 SNOWPLOWABLE RAISED PAVEMENT MARKERS (MONO-DIR) (1 COLOR)	EACH		15	15
	716-01.30 REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	684		684
(12)(13)	716-02.04 PLASTIC PAVEMENT MARKING (CHANNELIZATION STRIPING)	S.Y.	24		24
(12)(13)(14)	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	287	120	407
(12)(13)	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	4		4
(12)(13)	716-02.12 PLASTIC PAVEMENT MARKING (8IN LINE)	L.M.	0.1		0.1
(12)(13)	716-03.03 PLASTIC WORD PAVEMENT MARKING (STOP AHEAD)	EACH	1		1
(12)(13)	716-04.12 PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F.	24		24
(15)	716-05.20 PAINTED PAVEMENT MARKING (6" LINE)	L.M.	28.9		28.9
	716-09.86 CONTRAST PAVEMENT MARKING 6"	L.M.	0.1		0.1
	716-09.94 CONTRAST PAVEMENT SHADOW MARKING 6"	L.M.	0.1		0.1
(13)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.		28.9	28.9
	717-01 MOBILIZATION	LS	1		1

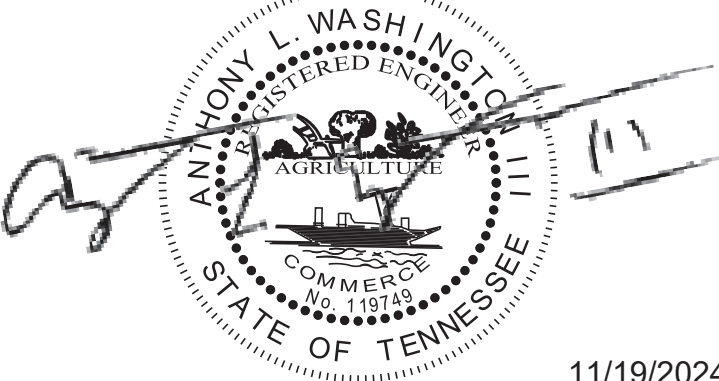
THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT

FOOTNOTES

- (1) TO BE USED AS DIRECTED BY THE ENGINEER. NO DIRT OR DEBRIS TO BE LEFT ON SHOULDER, THIS WORK TO BE PERFORMED BEFORE ALL OTHER OPERATIONS
- (2) INCLUDES 1,052 TONS FOR BREAKOUTS.
- (3) TO BE USED FOR BREAKOUTS ONLY
- (4) INCLUDES 4 TONS FOR DRIVEWAYS, COUNTY ROADS, FIELD ENTRANCES, BUSINESS ENTRANCES, AND EXTRA WIDTH PAVING.
- (5) USE CRAFTCO PAVEMENT JOINT ADHESIVE #34524, PAVON JOINT ADHESIVE BY PAVON CORPORATION, OR DENSO TAPE BY DENSO. FOR USE ON 411D JOINTS ONLY.
- (6) TO BE USED FOR SEALING ALL SURFACE LAYER CONSTRUCTION JOINTS ALONG THE TRAVEL LANES AND SHOULDERS AS DIRECTED BY THE ENGINEER.
- (7) INCLUDES 841 TONS FOR DRIVEWAYS, COUNTY ROADS, FIELD ENTRANCES, BUSINESS ENTRANCES, AND EXTRA WIDTH PAVING. INCLUDES 100 TONS FOR SPOT LEVELING.
- (8) INCLUDES 833 TONS FOR DRIVEWAYS, COUNTY ROADS, FIELD ENTRANCES, BUSINESS ENTRANCES, AND EXTRA WIDTH PAVING.
- (9) SEE GUARDRAIL TABULATION ON SHEET 2E.
- (10) THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. THIS ITEM INCLUDES TRAFFIC CONTROL FOR ANY BRIDGE REPAIR WORK.
- (11) THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION)
- (12) ITEM TO BE USED AS PERMANENT MARKING ONLY
- (13) THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- (14) ALL STOP BARS ON SIDE STREETS WILL BE RETRACED AS DIRECTED BY THE ENGINEER.
- (15) ITEM TO BE USED AS TEMPORARY PAVEMENT MARKING ONLY.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	HSIP-22(102)	2

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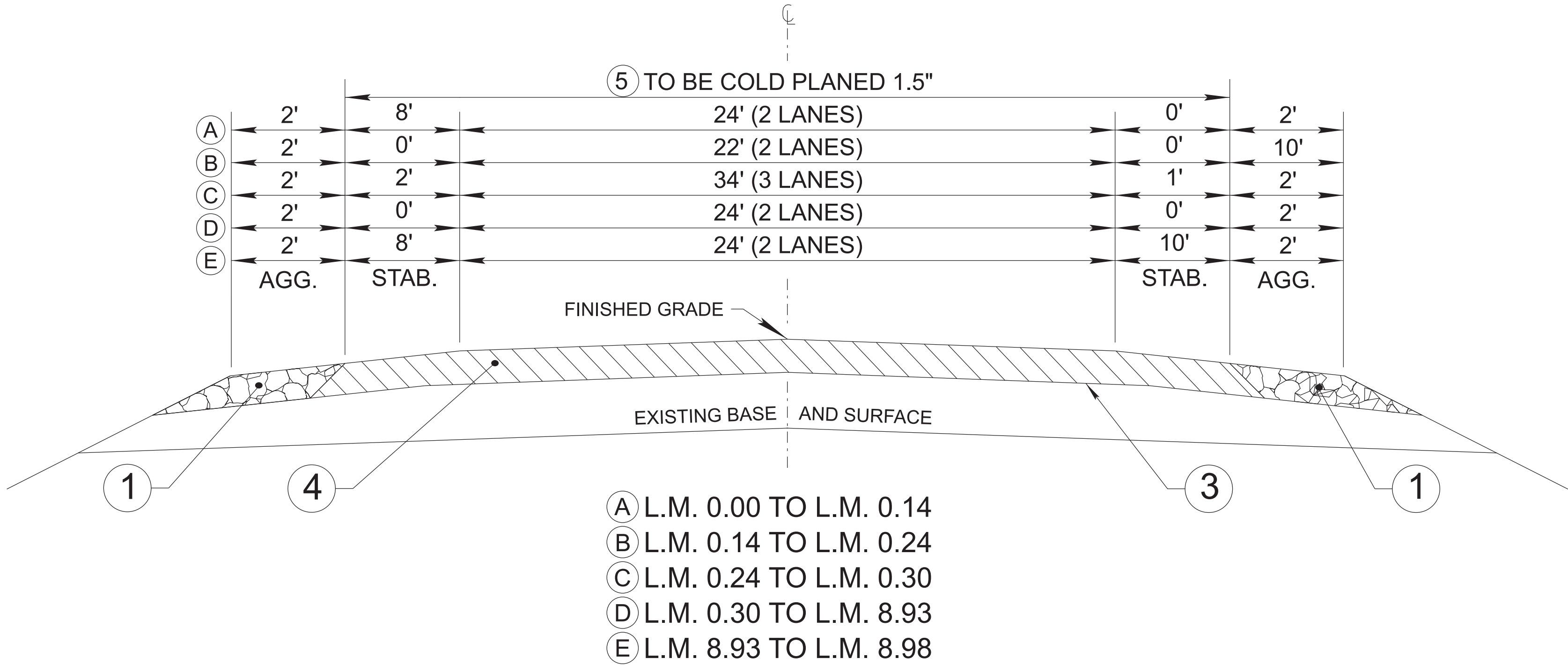


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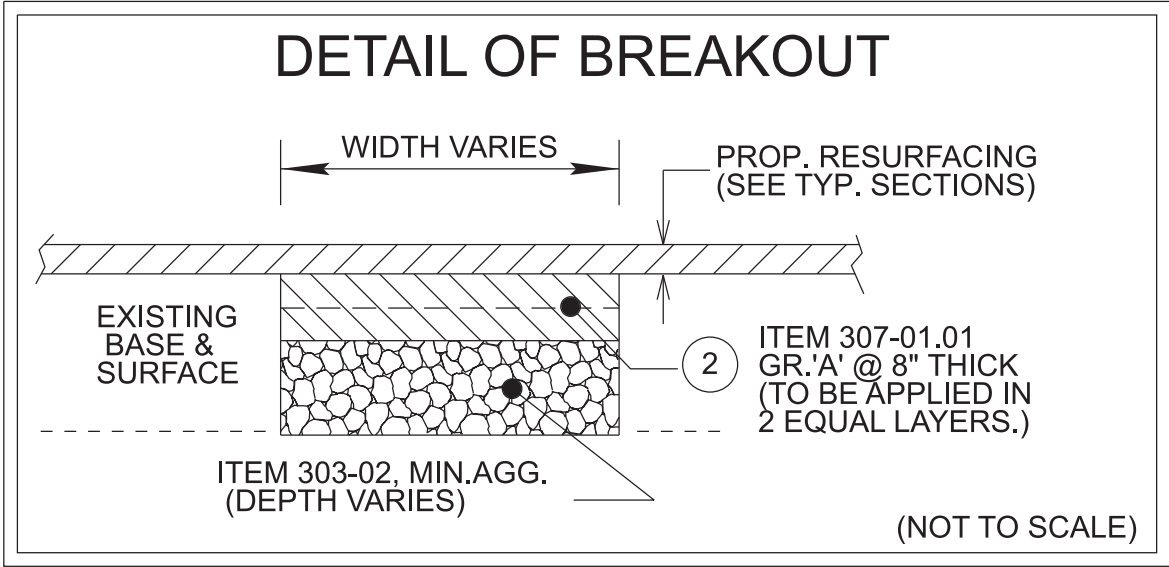
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	HSIP-22(102)	2B



PROPOSED PAVEMENT SCHEDULE	
① MINERAL AGGREGATE BASE @ 2.00" ± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "C OR D"	④ ASPHALTIC CONCRETE SURFACE (HOT MIX) @ PG70-22 GRADING "D" SURFACE @ 1.50" THICK (APPROX. 159.0 LB./S.Y.) 411-02.10 ACS MIX (PG70-22) GRADING "D"
② BITUMINOUS COURSE (BLACK BASE) @ 8.00"± THICK (APPROX. 920.0 LBS./S.Y.) ITEM 307-02.01 ASPHALT CONCRETE MIX (PG70-22)(BPMB-HM) GRADING "A" (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY)	⑤ COLD PLANING @ 1.50"± THICK (APPROX. 157.5 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT
③ TACK COAT (TC) ITEM NO. 403-02.01 TRACKLESS TACK COAT (SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD)	



BRIDGE NOTES				
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK NOTES
39SR0220001	1.280	DRAINAGE DITCH	38'-6"	PAVE WITH PLANS MIX/TREATMENT TYPE
39SR0220003	6.070	PINEY CREEK	142'-6"	SEAL WITH TYPE 1 THIN EPOXY OVERLAY
39SR0220005	8.580	POORHOUSE CREEK	26'	PAVE WITH PLANS MIX/TREATMENT TYPE

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NOT TO SCALE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

- (1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.
- (3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE “A” LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE “A” LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

MISCELLANEOUS

- (1) ALL DETOUR, ACCESS, SERVICE AND FRONTAGE ROADS SHALL BE CONSTRUCTED WITH A MINIMUM OF ONE (1) COURSE OF BASE MATERIAL BEFORE TRAFFIC IS INTERRUPTED ON EXISTING ROADS.
- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6” LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
- a. SHOULDERS SHALL BE BROOMED AND DE-GRASSED AND MATERIAL SHALL BE PICKED UP AND REMOVED. THIS WILL BE PAID FOR UNDER ITEM NO. 208-01.05.

b. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

- (9) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6” ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (20) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING


- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

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RESURF	2025	HSIP-22(102)	2C

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STATE OF TENNESSEE
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GENERAL
NOTES

SPECIAL NOTES

RESURFACING

- (1) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (2) WHEN A PERFORMANCE GRADE (PG) ASPHALT MIX WITH PROPERTIES GREATER THAN THAT OF PG64-22 IS CALLED FOR ON RESURFACING PLANS AND IS THE ONLY ASPHALT GRADE ON THE PROJECT, THE CONTRACTOR HAS THE OPTION OF USING EITHER THE ASPHALT GRADE SHOWN IN THE PLANS OR AN ASPHALT GRADE EQUAL TO OR BETTER THAN PG64-22 FOR DRIVEWAYS AND BUSINESS ENTRANCES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE MATERIAL WILL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THE ASPHALT OR ASPHALT MIX. THE MATERIAL TICKETS SHALL BE MARKED "FOR DRIVEWAYS AND BUSINESS ENTRANCES ONLY" AT THE POINT OF MANUFACTURE.
- (4) THE OUTSIDE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE TRAFFIC LANE.
- (6) THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 96 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED, EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.
- (8) FEATHER SURFACE MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.

PAVEMENT MARKING

- (1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.
- (2) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

SIGNS

- (1) IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS


- (1) ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.
- (4) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION. ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

JOINT SEALANTS

- (1) THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN ALL PAVEMENT LAYERS EXCEPT OGFC SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (2) PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSED AIR OR A POWER SWEEPER.
- (3) TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENTLY TO PREVENT DAMAGE FROM TRACKING.

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SPECIAL
NOTES

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5) SEE PROJECT COMMITMENTS, SHEET 1B FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES COLD PLANING EXISTING ASPHALT, RESURFACING, PAVEMENT MARKINGS, GUARDRAIL UPGRADES, TEMPORARY TRAFFIC CONTROL, AND BROOMING AND DEGRASSING SHOULDERS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

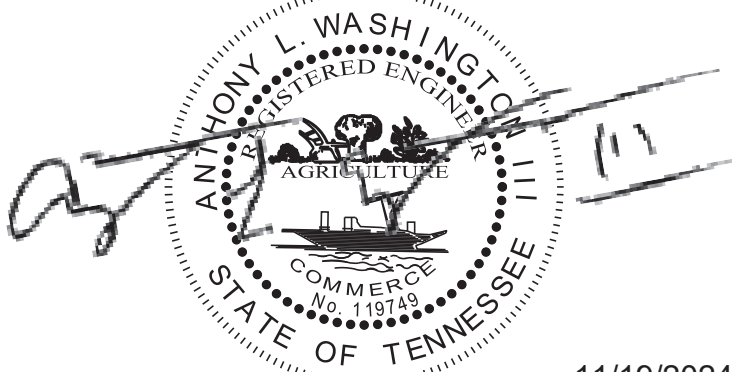
- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

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ENVIRONMENTAL
NOTES


ENVIRONMENTAL NOTES (CONT.)

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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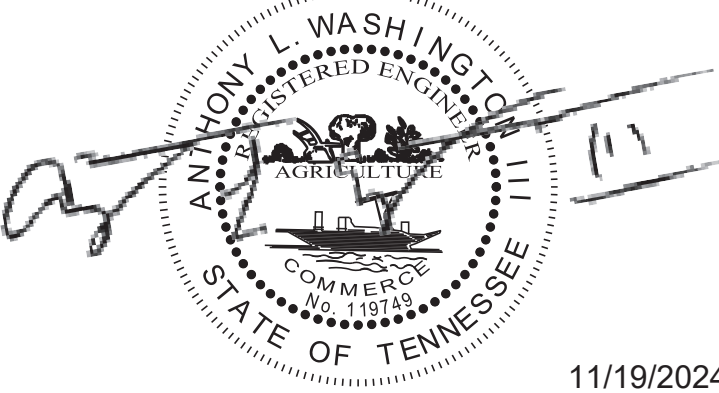
ENVIRONMENTAL
NOTES

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)					
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES L x W	S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
G20-1	ROAD WORK NEXT 9 MILES	48" x 24"	8	2	16
G20-2	END ROAD WORK	48" x 24"	8	25	200
W8-11	UNEVEN LANES	48" x 48"	16	47	752
W8-15	GROOVED PAVEMENT	48" x 48"	16	47	752
W8-15P	MOTORCYCLE PLAQUE	30" x 24"	5	47	235
W20-1	ROAD WORK 1 MILE	48" x 48"	16	2	32
W20-1	ROAD WORK 1/2 MILE	48" x 48"	16	2	32
W20-1	ROAD WORK 1000 FT	48" x 48"	16	2	32
W20-1	ROAD WORK AHEAD	48" x 48"	16	23	368
W20-4	ONE LANE ROAD 1000 FT - PORTABLE	48" x 48"	16	2	32
W20-7A	FLAGGER SYMBOL - PORTABLE	48" x 48"	16	2	32
W21-2	FRESH OIL - PORTABLE	48" x 48"	16	2	32
W21-5	SHOULDER WORK	48" x 48"	16	4	64
THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.				TOTAL	2579

PROPOSED GUARDRAIL (RESURFACING)						
SIDE		LOG MILE	GUARDRAIL			TERMNAL ANCHORS
			GUARDRAIL BEAM TRANSITION 27 IN TO 31 IN 705-02.10 (EACH)	EARTH PAD FOR GUARD RAIL END TREATMENT 705-04.10 (EACH)	GUARDRAIL REMOVED 706-01 (L.F.)	TYPE 38 MASH TL3 (26.896') 705-06.20 (EACH)
LT	RT					
X		0.000	1	1	50	1
X		0.825	1	1	50	1
	X	0.866	1	1	50	1
X		0.932	1	1	50	1
	X	1.236	1	1	50	1
X		1.258	1	1	50	1
	X	1.301	1	1	50	1
X		1.303	1	1	50	1
X		1.581	1	1	50	1
X		1.963	1	1	50	1
X		2.504	1	1	50	1
	X	2.558	1	1	50	1
X		2.642	1	1	50	1
	X	5.566	1	1	50	1
	X	6.045	1	1	50	1
X		6.052	1	1	50	1
	X	7.222	1	1	50	1
TOTALS			17	17	850	17

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TABULATED
QUANTITIES

UTILITY NOTES

UTILITY

- (2)

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3)

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4)

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5)

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

COMMUNICATIONS:

AT&T

315 E. COLLEGE STREET
JACKSON, TN 38301
CONTACT: DANIEL R. POTTS
OFFICE PHONE: 901 488 2359
Email: DP7607@ATT.COM

CABLE:

CHARTER COMMUNICATIONS

24 CIRCLE DRIVE
MCKENZIE, TN 38201
CONTACT: KEITH CHESSER
OFFICE PHONE: 731 352 1146
CELL PHONE: 731 621 9552
Email: KEITH.CHESSER@CHARTER.COM

ELECTRIC:

LEXINGTON ELECTRIC SYSTEM

92 S. MAIN ST.
LEXINGTON, TN 38351
CONTACT: BRAD JONES
OFFICE PHONE: 731 967 7319
Email: BJONES@LEXINGTONELECTRIC.COM


GAS / WATER / SEWER:

LEXINGTON UTILITIES

54 MONROE ST.
LEXINGTON, TN 38351
CONTACT: MICHAEL HARPER
OFFICE PHONE: 731 968 5213
Email: MICHAEL.HARPER@LEXINGTONTN.GOV

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	HSIP-22(102)	3

SEALED BY



11/19/2024

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES
AND
UTILITY OWNERS

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a.

WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b.

IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

a.

THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.
- b.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.

c.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.

d.

THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.
- FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
4.

FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.
- IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
- B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:
1.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

a.

WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b.

ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2.

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	HSIP-22(102)	4

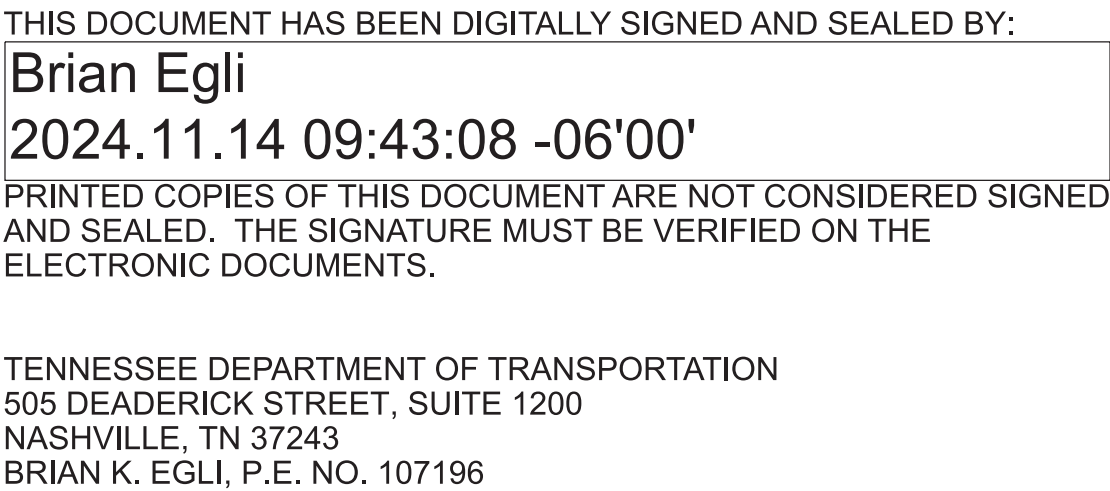
SEALED BY

11/19/2024

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

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SHEET NAME	SHEET NO.
SIGNATURE SHEET	STRUCTURE-SIGN
BRIDGE PLANS	B1 THRU B5

YEAR	PROJECT NO.	SHEET NO.
2025	39S022-M3-004	STRUCTURE-SIGN 1
STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION		
SIGNATURE SHEET		

PIN 135283.00

[illegible]

INDEX OF DRAWINGS	DWG. NO.	LAST REV. DATE
SIGNATURE SHEET	STRUCTURE-SIGN 1	
INDEX OF DRAWINGS	B1	
BRIDGE TABULATION AND ESTIMATED QUANTITIES	B2	
TYPE I THIN EPOXY OVERLAY NOTES	B3	
PLAN VIEW (39SR0220003)	B4	
PHASE CONSTRUCTION	B5	

INDEX OF REFERENCE DRAWINGS	DWG. NO.
LAYOUT OF BRIDGE_____	F-3-126
BRIDGE REPAIR DETAILS_____	BR-48-29

DEPARTMENT OF TRANSPORTATION
INDEX OF DRAWINGS
39-SR22-6.07
OVER
PINEY CREEK
BR. NO. 39SR0220003
HENDERSON COUNTY
2025

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 6/24
 SUPERVISED BY K. MARTINKO DATE 6/24
 CHECKED BY _____ DATE _____

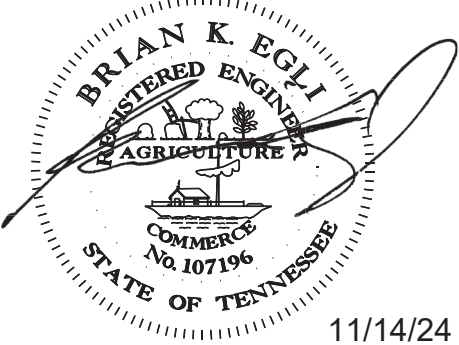
PIN 135283.00

PROJECT NO.		YEAR	SHEET NO.
39S022-M3-004		2025	B2
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES			
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	617-04.01 TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
39-SR22-6.07 OVER PINEY CREEK (39SR0220003)	F-3-126 BR-48-29	TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	491
TOTAL			491

TABULATED TRAFFIC CONTROL QUANTITIES			
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	150
712-05.01	WARNING LIGHTS (TYPE A)	EACH	4
712-05.03	WARNING LIGHTS (TYPE C)	EACH	40
712-06	SIGNS (CONSTRUCTION)	S.F.	14
713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	2

CONSTRUCTION SIGNS					
DESCRIPTION	NO. REQ'D.	SIZE	SIGN I.D. NO.	QUANTITY	UNIT
ONE LANE ROAD AHEAD	2	48 X 48	W20-4	32	S.F.
FLAGGER SYMBOL	2	48 X 48	W20-7A	32	S.F.
ITEM 712-06 SIGNS (CONSTRUCTION)				64.0	S.F.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE TABULATION AND
ESTIMATED QUANTITIES
39-SR22-6.07
OVER
PINEY CREEK
BR. NO. 39SR0220003
HENDERSON COUNTY
2025

DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 6/24
SUPERVISED BY K. MARTINKO DATE 6/24
CHECKED BY _____ DATE _____

[illegible]

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

APPLICATION EQUIPMENT SHOULD :

- F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

<u>SIEVE SIZE</u>	<u>% PASSING</u>
-------------------	------------------

NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER'S AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS.

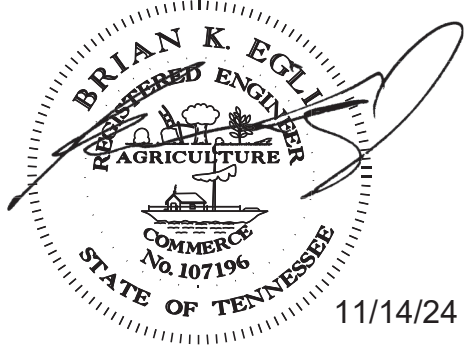
BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), SY.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, ETC., BUT IN ALL CASES, ANY DESTRUCTIVELY TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
TYPE I THIN EPOXY
OVERLAY NOTES
39-SR22-6.07
OVER
PINEY CREEK
BR. NO. 39SR0220003
HENDERSON COUNTY
2025

[illegible]

PLAN VIEW
39-SR22-6.07
OVER
PINEY CREEK
BR. NO. 39SR0220003
HENDERSON COUNTY
2025

B4

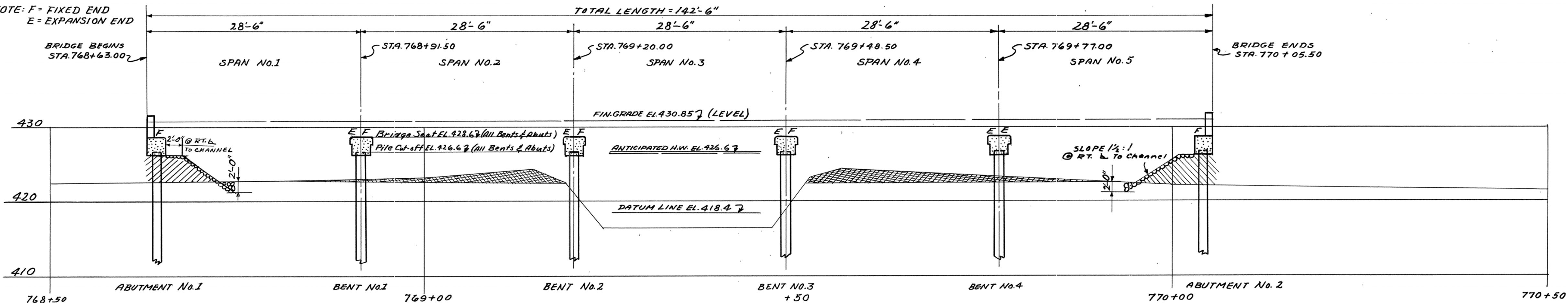
PROJECT NO.	YEAR	SHEET NO.
39S022-M3-004	2025	B5

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NORTHBOUND BRIDGE
(39-SR22-6.07)

B5

NOTE: F = FIXED END
E = EXPANSION END



SECTION ALONG & OF BRIDGE AND ROADWAY

NOTE: ROADWAY CONTRACTOR TO REMOVE TREES AND BRUSH FROM CHANNEL AND EXCAVATE CROSS-HATCHED AREA TO EL. 422.5 AS SHOWN ABOVE FOR 200' UPSTREAM & 200' DOWNSTREAM.

NOTE: FILL UNDER ABUTMENTS No. 1 & No. 2 TO BE IN PLACE AND PROPERLY COMPACTED BEFORE PILES ARE DRIVEN.

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
8	TENN.		19	17	99

F-022-3-11

DESIGN DATA

DRAINAGE AREA 7365 ACRES
A FACTOR OF C=8 IN TALBOTS FORMULA
GIVES: WATER AREA REQUIRED 637 SQ. FT.
WATER AREA PROVIDED 680 SQ. FT.
At RT. L To Channel

GENERAL NOTES

SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF HIGHWAYS AND PUBLIC WORKS.

LOADING: H20-44.

CONCRETE: TO BE CL. "A"

REINFORCING STEEL: MAY BE EITHER BILLET STEEL MEETING ASTM SPECIFICATIONS A15-52T OR RAIL STEEL MEETING ASTM SPECIFICATIONS A16-52T.

FORMS AND FINISH: SEE SPECIFICATIONS.

RIP-RAP: SEE SPECIFICATIONS.

PILES: TO BE PRECAST CONCRETE.

LIST OF DWGS.

DWG. NO.

HANDRAIL (Also see Handrail Note on this Sheet) E-12-105
* DECK GIRDERS - 5 - 26' CLEAR SPANS E-8-54
SKEWED SLAB DETAILS (75° SKEW LT.) E-12-24
ABUTMENTS No. 1 & No. 2 (PILE BENT TYPE) F-2-41
BENTS No. 1, No. 2, No. 3 & No. 4 (PILE BENT TYPE) F-2-41

* BRONZE PLATES SHALL BE REPLACED BY STRUCTURAL STEEL PLATES ON THIS BRIDGE. SURFACES OF THE PLATES THAT COME IN CONTACT SHALL BE COATED WITH GRAPHITE IN THE FIELD.
ALSO, 3" TRANSITE DRAINS OR EQUAL TO BE USED IN LIEU OF 4" C.I. DRAINS. OMIT DRAINS OVER RIP-RAP AREA.
42 DRAINS (1'-0" LONG) REQUIRED.

28'-0" ROADWAY WITH SAFETY CURBS

STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
AND PUBLIC WORKS

NASHVILLE
LAYOUT OF BRIDGE
OVER PINEY CR.
STA. 768+63.00
HENDERSON COUNTY
1954

ESTIMATED QUANTITIES

ITEM	CONCRETE CL. "A" CU. YDS.	STEEL - LBS.		CONCRETE TEST PILES LIN. FT.	PRECAST CONCRETE PILES SIZE 1'-11 1/2" LIN. FT.	RIP-RAP CU. YDS.
		REINFORC.	STRUCT.			
SUPERSTRUCTURE	14.7	2800	—	—	—	—
DECK GIRDERS	205.1	48,165	1845	—	—	—
ABUTMENT No. 1	11.0	1,206	—	—	200	46
BENT No. 1	7.2	873	—	—	200	—
BENT No. 2	7.2	873	—	—	200	—
BENT No. 3	7.2	873	—	—	200	—
BENT No. 4	7.5	848	—	—	200	—
ABUTMENT No. 2	11.0	1,206	—	—	200	51
TOTALS	270.9	56,844	1845	40	1200	97

Note: Cost of Roadway Drains & Joint Material To Be Included In Unit cost of Class "A" Concrete.

SPECIAL NOTE: TEST PILES

BEFORE ANY PILES ARE ORDERED, A PRECAST CONCRETE TEST PILE 40 FT. LONG, SHALL BE DRIVEN AT STA. 769+25. FROM THE RESULTS OBTAINED, PILES SHALL BE ORDERED OF SUCH LENGTH AS TO PROVIDE A MINIMUM BEARING OF 21 TONS FOR THE ABUTMENTS AND 30 TONS FOR THE BENTS.

HANDRAIL NOTE

BUILD ACCORDING TO STD. DWG. E-12-105 EXCEPT FOR THE FOLLOWING:

SPAN No. 1 (END SPAN)

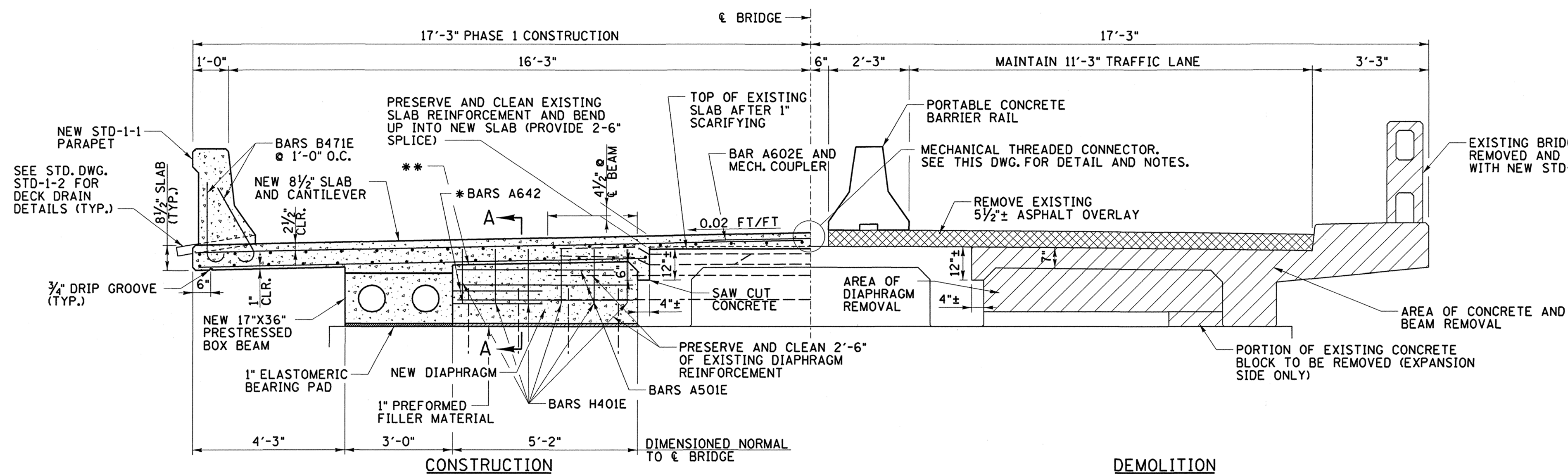
SHORT SIDE: M=28'-4" USE 4 LG 4'-11 1/2" - X" 2"

LONG SIDE: M=29'-4 1/2" USE 4 LG 5'-2 1/2" MAKE "X" 1/2" & MAKE 8 BARS F 4'-10" LONG & 16 BARS G 5'-4" LONG.

SPAN No. 5: SAME AS SPAN No. 1

SPANS No. 2, No. 3 & No. 4: (INTERMEDIATE SPANS) SAME AS ON STD. DWG. FOR 26' CLEAR INTERMEDIATE SPAN.

DESIGNED BY: G. ZASSO
DRAWN BY: A. KELLEY
TRACED BY: A. KELLEY
CHECKED BY: _____
DATE: 7-16-54
DATE: 7-22-54
DATE: _____

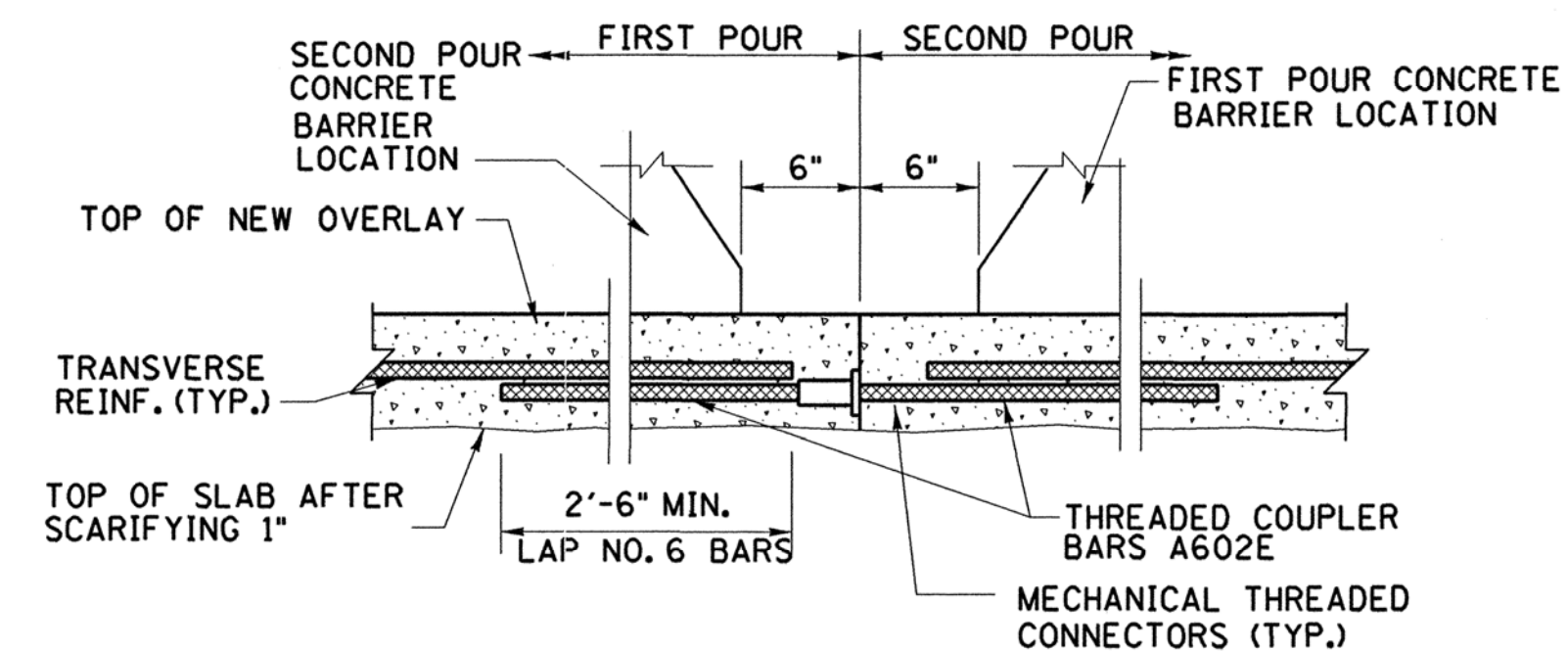
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NOTES:
SEE DWG. NO. BR-48-31 FOR
SECTION A-A.

* BARS A642 TO BE INCLUDED IN
ITEM NO. 615-02.02, PRESTRESSED
CONCRETE BOX BEAM (17"x36"), L.F.
SEE DWG. NO. BR-48-32.

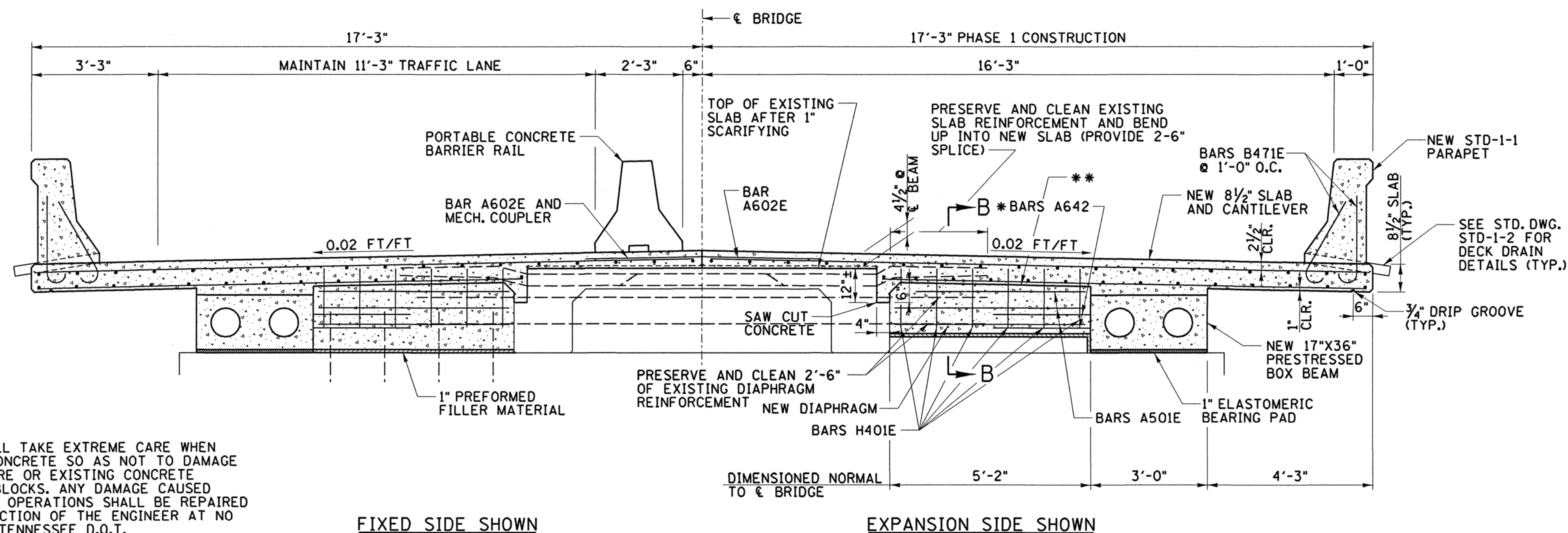
SECTION
(SHOWING PHASE 1 OF
SLAB OVERLAY DETAIL)
SCALE: 1/2" = 1'-0"

CONSTRUCTION JOINT REQ'D
BETWEEN TOP OF DIAPHRAGM
AND SLAB.



BAR CONNECTOR DETAIL
N.T.S.

SHOWING TRANSVERSE REINFORCING STEEL SPLICING
WITH MECHANICAL THREADED CONNECTORS.
COST OF MECHANICAL THREADED CONNECTORS TO BE
INCLUDED UNDER ITEM NO. 604-10.18, REINFORCING
STEEL (REPAIRS), LBS.



NOTE:
THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN
REMOVING EXISTING CONCRETE SO AS NOT TO DAMAGE
EXISTING SUBSTRUCTURE OR EXISTING CONCRETE
PEDESTALS AND END BLOCKS. ANY DAMAGE CAUSED
BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED
TO THE FULL SATISFACTION OF THE ENGINEER AT NO
ADDITIONAL COST TO TENNESSEE D.O.T.

FIXED SIDE SHOWN

EXPANSION SIDE SHOWN

TYPICAL SECTION AT BENT
(SHOWING PHASE 2 OF
SLAB OVERLAY DETAIL)
SCALE: 1/2" = 1'-0"

NOTES:
THE FINISHED GRADE OF THE CONCRETE ROADWAY CENTERLINE SHALL BE PARALLEL TO THE GRADE OF THE EXISTING BRIDGE. THE NEW SLAB SURFACE SHALL HAVE CROSS SLOPE OF 0.02 FT/FT. THE LONGITUDINAL GRADE AND CROSS SLOPE SHALL BE PROFILED USING METHODS SUITABLE FOR NEW CONSTRUCTION.

COST OF EPOXY COATED REINFORCING STEEL SHALL BE PAID FOR UNDER ITEM NO.604-10.18, REINFORCING STEEL (REPAIRS).LBS.

THE CONCRETE DIAPHRAGMS SHALL BE POURED PRIOR TO POURING THE SLAB, CANTILEVER AND BRIDGE DECK OVERLAY. THE DIAPHRAGM CONCRETE SHALL ATTAIN A MIN. COMPRESSIVE STRENGTH OF 3000 PSI BEFORE THE DECK CONCRETE IS POURED.

SLAB AND CANTILEVER REPLACEMENT SHALL BE POURED MONOLITHICALLY WITH THE CONCRETE BRIDGE DECK OVERLAY. ALL EXPOSED REINFORCEMENT TO REMAIN SHALL BE CAREFULLY PRESERVED AND COMPLETELY CLEANED TO REMOVE ALL RUST.

THE COST OF FORMING AND POURING NEW HIGH EARLY STRENGTH CONCRETE FOR OVERLAY, CANTILEVERS AND DIAPHRAGMS SHALL BE INCLUDED UNDER ITEM NO. 604-10.09, CONCRETE, C.Y.

ALL COSTS ASSOCIATED WITH SCARIFYING OF EXISTING BRIDGE DECK 1" SHALL BE INCLUDED IN
ITEM NO. 604-10.51, SCARIFYING, S.Y.

COST OF REMOVING EXISTING CANTILEVERS, BRIDGERAIL, EXTERIOR BEAMS, SLAB, DIAPHRAGMS, CLEANING EXISTING REINFORCING STEEL TO REMAIN, SAW CUTTING AND ALL LABOR AND ANY MISCELLANEOUS ITEMS FOR DEMOLITION AS SHOWN ON THE DETAILS SHALL BE INCLUDED IN ITEM NO. 604-10.13, CONCRETE SLAB REMOVAL, L.S.

ALL COSTS ASSOCIATED WITH THE INTERCONNECTED PORTABLE BARRIER RAIL SHALL BE INCLUDED IN ITEM NO. 712-02.02, INTERCONNECTED PORTABLE BARRIER RAIL, L.F.

FOR MORE DETAILS OF TRAFFIC CONTROL, SEE SHEET NO. 4.

ASPHALT REMOVAL FROM BRIDGE END TO BRIDGE END SHALL BE PAID FOR UNDER ITEM NO.
604-10.14, REMOVE
EXISTING WEARING SURFACE, L.S.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS

STATE ROUTE 22 OVER
PINEY CREEK
BRIDGE NO. 39-SR022-06.07

HENDERSON COUNTY
2000



DESIGNED BY A. J. KHAIRI DATE AUG. 2000
 DRAWN BY K. KYZER DATE AUG. 2000
 SUPERVISED BY J. H. RUDDELL DATE AUG. 2000
 CHECKED BY A. J. KHAIRI DATE AUG. 2000

TN D.O.T. ENGINEERING SUPERVISOR M. LAWSON

BR-48-29